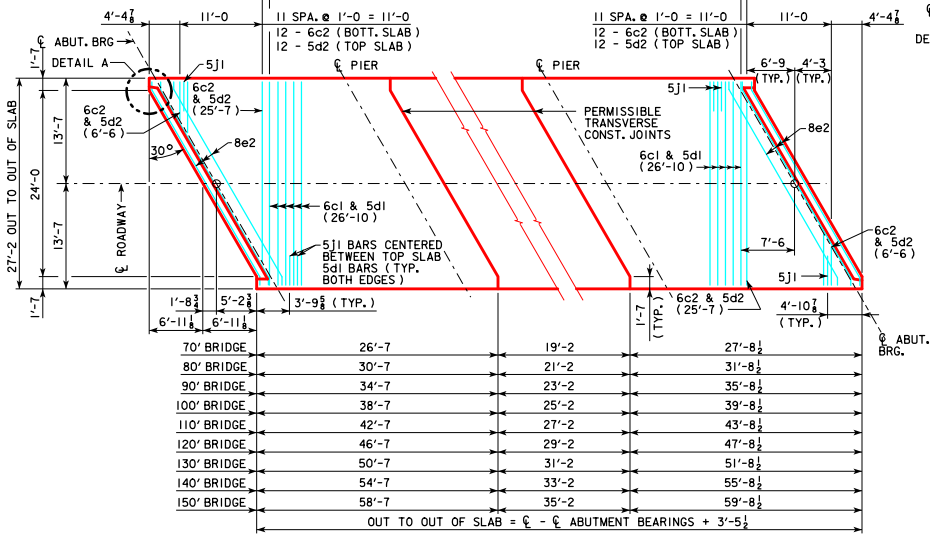


REVISED 12-08 - REVISED PAVING NOTCH, TRANSVERSE REINFORCING AND ADDED DETAIL B. ADDED TEMPORARY PAVING BLOCK DETAIL. REVISED 06-2022: UPDATED BRIDGE ENGINEER SIGNATURE. CHANGED PAVING BLOCK LIFTING HOOP BAR MARK, (WAS 5x1).

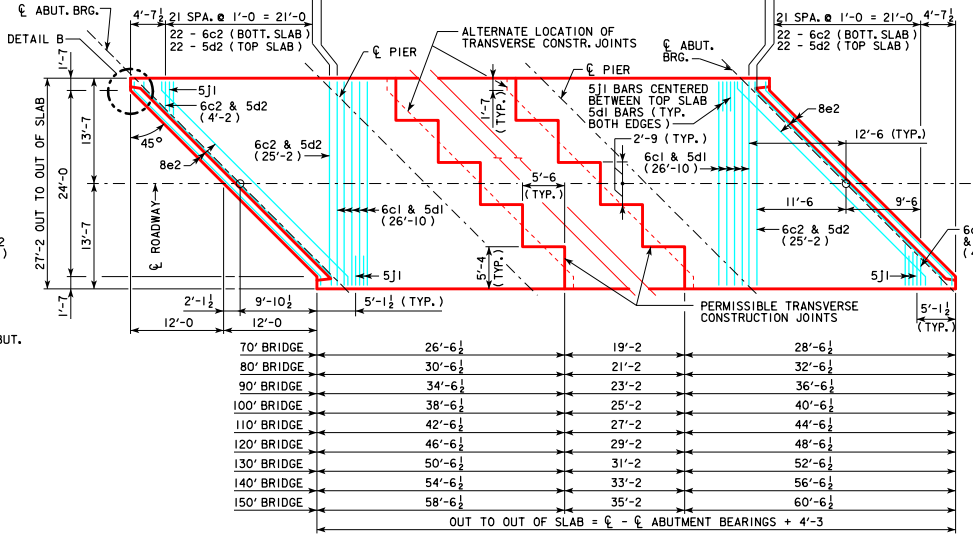
70' BRIDGE	9	55 SPA. @ 1'-0" = 55'-0" 56-6c1 (BOTTOM SLAB) & 56-5d1 (TOP SLAB)	9
80' BRIDGE	9	65 SPA. @ 1'-0" = 65'-0" 66-6c1 (BOTTOM SLAB) & 66-5d1 (TOP SLAB)	9
90' BRIDGE	9	75 SPA. @ 1'-0" = 75'-0" 76-6c1 (BOTTOM SLAB) & 76-5d1 (TOP SLAB)	9
100' BRIDGE	9	85 SPA. @ 1'-0" = 85'-0" 86-6c1 (BOTTOM SLAB) & 86-5d1 (TOP SLAB)	9
110' BRIDGE	9	95 SPA. @ 1'-0" = 95'-0" 96-6c1 (BOTTOM SLAB) & 96-5d1 (TOP SLAB)	9
120' BRIDGE	9	105 SPA. @ 1'-0" = 105'-0" 106-6c1 (BOTTOM SLAB) & 106-5d1 (TOP SLAB)	9
130' BRIDGE	9	115 SPA. @ 1'-0" = 115'-0" 116-6c1 (BOTTOM SLAB) & 116-5d1 (TOP SLAB)	9
140' BRIDGE	9	125 SPA. @ 1'-0" = 125'-0" 126-6c1 (BOTTOM SLAB) & 126-5d1 (TOP SLAB)	9
150' BRIDGE	9	135 SPA. @ 1'-0" = 135'-0" 136-6c1 (BOTTOM SLAB) & 136-5d1 (TOP SLAB)	9



**30° SKEW**

70' BRIDGE	26'-7"	19'-2"	27'-8 1/2"
80' BRIDGE	30'-7"	21'-2"	31'-8 1/2"
90' BRIDGE	34'-7"	23'-2"	35'-8 1/2"
100' BRIDGE	38'-7"	25'-2"	39'-8 1/2"
110' BRIDGE	42'-7"	27'-2"	43'-8 1/2"
120' BRIDGE	46'-7"	29'-2"	47'-8 1/2"
130' BRIDGE	50'-7"	31'-2"	51'-8 1/2"
140' BRIDGE	54'-7"	33'-2"	55'-8 1/2"
150' BRIDGE	58'-7"	35'-2"	59'-8 1/2"
OUT TO OUT OF SLAB = $\phi$ - $\phi$ ABUTMENT BEARINGS + 3'-5 1/2"			

70' BRIDGE	1'-0"	45 SPA. @ 1'-0" = 45'-0" 46-6c1 (BOTT. SLAB) & 46-5d1 (TOP SLAB)	1'-0"
80' BRIDGE	1'-0"	55 SPA. @ 1'-0" = 55'-0" 56-6c1 (BOTT. SLAB) & 56-5d1 (TOP SLAB)	1'-0"
90' BRIDGE	1'-0"	65 SPA. @ 1'-0" = 65'-0" 66-6c1 (BOTT. SLAB) & 66-5d1 (TOP SLAB)	1'-0"
100' BRIDGE	1'-0"	75 SPA. @ 1'-0" = 75'-0" 76-6c1 (BOTT. SLAB) & 76-5d1 (TOP SLAB)	1'-0"
110' BRIDGE	1'-0"	85 SPA. @ 1'-0" = 85'-0" 86-6c1 (BOTT. SLAB) & 86-5d1 (TOP SLAB)	1'-0"
120' BRIDGE	1'-0"	95 SPA. @ 1'-0" = 95'-0" 96-6c1 (BOTT. SLAB) & 96-5d1 (TOP SLAB)	1'-0"
130' BRIDGE	1'-0"	105 SPA. @ 1'-0" = 105'-0" 106-6c1 (BOTT. SLAB) & 106-5d1 (TOP SLAB)	1'-0"
140' BRIDGE	1'-0"	115 SPA. @ 1'-0" = 115'-0" 116-6c1 (BOTT. SLAB) & 116-5d1 (TOP SLAB)	1'-0"
150' BRIDGE	1'-0"	125 SPA. @ 1'-0" = 125'-0" 126-6c1 (BOTT. SLAB) & 126-5d1 (TOP SLAB)	1'-0"



**45° SKEW**

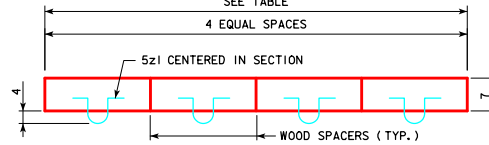
70' BRIDGE	26'-6 1/2"	19'-2"	28'-6 1/2"
80' BRIDGE	30'-6 1/2"	21'-2"	32'-6 1/2"
90' BRIDGE	34'-6 1/2"	23'-2"	36'-6 1/2"
100' BRIDGE	38'-6 1/2"	25'-2"	40'-6 1/2"
110' BRIDGE	42'-6 1/2"	27'-2"	44'-6 1/2"
120' BRIDGE	46'-6 1/2"	29'-2"	48'-6 1/2"
130' BRIDGE	50'-6 1/2"	31'-2"	52'-6 1/2"
140' BRIDGE	54'-6 1/2"	33'-2"	56'-6 1/2"
150' BRIDGE	58'-6 1/2"	35'-2"	60'-6 1/2"
OUT TO OUT OF SLAB = $\phi$ - $\phi$ ABUTMENT BEARINGS + 4'-3"			

**TRANSVERSE REINFORCING STEEL LAYOUT**

NOTE: 5d2 BARS ARE TO PASS UNDER 8e2 BARS IN CONFLICT AREAS ON 30° & 45° SKEW BRIDGES.

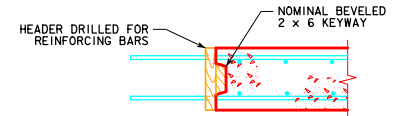
TEMPORARY PAVING BLOCK		
SKEW	LENGTH	CONCRETE
30°	25'-9"	0.6 C.Y.
45°	31'-11"	0.7 C.Y.

SEE TABLE

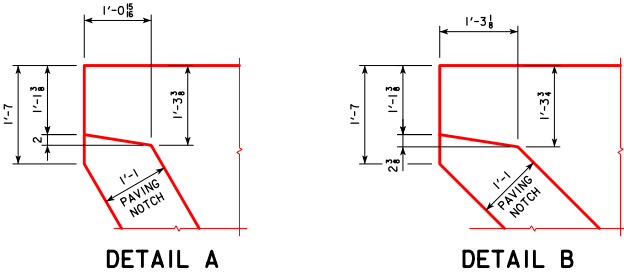


**TEMPORARY PAVING BLOCK DETAIL**

NOTE: TEMPORARY PAVING BLOCK TO BE USED WITH PAVED APPROACHES ONLY. LINE NOTCH WITH TAR PAPER BEFORE PLACING TEMPORARY PAVING BLOCK.



**TRANSVERSE CONSTRUCTION JOINT**



**DETAIL A**

**DETAIL B**

08-2022 LATEST REVISION DATE	APPROVED BY BRIDGE ENGINEER	<b>IOWA DOT</b>	
		STANDARD DESIGN - 24'-0" ROADWAY, 3 SPAN BRIDGES	
		<b>CONTINUOUS CONCRETE SLAB BRIDGES</b> NOVEMBER, 2006	
<b>SUPERSTRUCTURE DETAILS ALL BRIDGES</b>		<b>J24-22-06</b>	
30° & 45° SKEW			