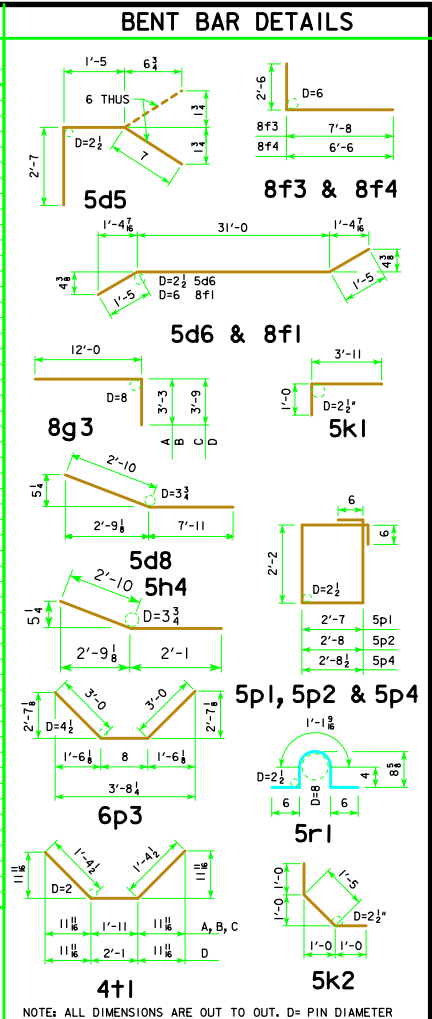


REVISED 09-14 - CORRECTED THE BARRIER RAIL REINFORCING STEEL QUANTITIES FOR ALL THE BRIDGE LENGTHS.  
 REVISED 05-15 - CORRECTED THE CONCRETE QUANTITIES OF THE PAVING BLOCKS FOR ALL THE BRIDGE LENGTHS. (WAS L5 CLYD.FOR 46'-8 THRU 67'-6 & L6 CLYD.FOR 80'-0 THRU 110'-0)  
 REVISED 08-15 - REMOVED THE NOTE "NON-EPOXY COATED" FROM THE "REINFORCING STEEL (LBS.)" LINE IN THE REINFORCING BAR CHART LIST.

REINFORCING BAR LIST			BRIDGE LENGTH																						
			46'-8			55'-0			67'-6			80'-0			90'-0			100'-0			110'-0				
BAR	LOCATION	SHAPE	NO.	LENGTH	WEIGHT	NO.	LENGTH	WEIGHT	NO.	LENGTH	WEIGHT	NO.	LENGTH	WEIGHT	NO.	LENGTH	WEIGHT	NO.	LENGTH	WEIGHT	NO.	LENGTH	WEIGHT		
ONE SUPERSTRUCTURE AND TWO ABUTMENTS																									
601	SLAB TRANSVERSE, TOP & BOTTOM		111	32'-10	5475	133	32'-10	6559	165	32'-10	8138	199	32'-10	9814	225	32'-10	11,097	253	32'-10	12,477	279	32'-10	13,760		
602	SLAB TRANSVERSE END, TOP		20	*	511	20	*	511	20	*	511	20	*	511	20	*	511	20	*	511	20	*	511		
603	SLAB TRANSVERSE END, BOTTOM		22	**	562	22	**	562	22	**	562	22	**	562	22	**	562	22	**	562	22	**	562		
5b1	SLAB LONGITUDINAL TOP & BOTTOM		168	25'-10	4527	168	30'-0	5257	168	36'-3	6352	252	29'-1	7644	252	32'-5	8520	252	35'-9	9396	252	39'-1	10,273		
5d2	ABUTMENT DIAPHRAGM, LONGITUDINAL - F.F.		24	6'-4	159	24	6'-4	159	24	6'-4	159	24	6'-1	153	24	6'-3	157	24	6'-3	157	24	6'-3	157		
5d3	ABUTMENT DIAPHRAGM, LONGITUDINAL - F.F.		8	5'-5	46	8	5'-5	46	8	5'-5	46	8	5'-2	44	8	5'-0	42	8	5'-0	42	8	5'-0	42		
5d5	ABUTMENT DIAPHRAGM, LONGITUDINAL - END		12	4'-7	58	12	4'-7	58	12	4'-7	58	12	4'-7	58	12	4'-7	58	12	4'-7	58	12	4'-7	58		
5d6	ABUTMENT DIAPHRAGM, LONGITUDINAL - B.F.		8	33'-10	283	8	33'-10	283	8	33'-10	283	8	33'-10	283	8	33'-10	283	8	33'-10	283	8	33'-10	283		
5d7	PAVING NOTCH, LONGITUDINAL		4	33'-11	142	4	33'-11	142	4	33'-11	142	4	33'-11	142	4	33'-11	142	4	33'-11	142	4	33'-11	142		
5d8	ABUTMENT DIAPH. WING EXT. LONGIT.		-	-	-	-	-	-	-	-	-	-	-	-	24	10'-9	269	24	10'-9	269	24	10'-9	269		
5d9	ABUT. DIAPH. WING EXT. LONGIT.		-	-	-	-	-	-	-	-	-	-	-	-	24	10'-8	267	24	10'-8	267	24	10'-8	267		
8f1	ABUTMENT FOOTING LONGITUDINAL		18	33'-10	1626	18	33'-10	1626	18	33'-10	1626	18	33'-10	1626	18	33'-10	1626	18	33'-10	1626	18	33'-10	1626		
8f3	ABUTMENT EXTENSION LONGITUDINAL		-	-	-	-	-	-	-	-	-	-	-	-	16	10'-2	435	16	10'-2	435	16	10'-2	435		
8f4	ABUTMENT EXTENSION LONGITUDINAL		-	-	-	-	-	-	-	-	-	-	-	-	16	9'-0	385	16	9'-0	385	16	9'-0	385		
8g1	ABUTMENT VERTICAL		114	6'-7	2004	114	6'-7	2004	114	7'-2	2181	110	7'-8	2252	110	8'-5	2472	110	8'-5	2472	110	8'-5	2472		
8g3	ABUTMENT DIAPHRAGM VERTICAL - B.F.		50	15'-3	2036	50	15'-3	2036	50	15'-3	2036	50	15'-9	2103	50	15'-9	2103	50	15'-9	2103	50	15'-9	2103		
6g4	ABUT. DIAPH. WING EXT. VERT.		-	-	-	-	-	-	-	-	-	-	-	-	40	5'-8	341	40	6'-5	386	40	6'-5	386		
5h1	ABUTMENT TO WING ANCHOR		28	6'-8	196	28	6'-8	196	28	6'-8	196	36	6'-8	252	36	6'-8	252	36	6'-8	252	36	6'-8	252		
5h2	ABUTMENT TO WING ANCHOR		56	4'-11	288	56	4'-11	288	56	4'-11	288	12	4'-11	62	12	4'-11	62	12	4'-11	62	12	4'-11	62		
5h3	ABUTMENT TO WING ANCHOR		28	6'-9	198	28	6'-9	198	28	6'-9	198	36	6'-9	256	36	6'-9	256	36	6'-9	256	36	6'-9	256		
5h4	ABUTMENT TO WING ANCHOR		-	-	-	-	-	-	-	-	-	-	-	-	12	4'-11	62	12	4'-11	62	12	4'-11	62		
5j1	SLAB TRANSV. TOP AT RAIL		128	6'-3	835	150	6'-3	978	182	6'-3	1187	216	6'-3	1409	242	6'-3	1578	270	6'-3	1761	296	6'-3	1930		
5k1	PAVING NOTCH, TRANSVERSE		54	4'-11	277	54	4'-11	277	54	4'-11	277	54	4'-11	277	54	4'-11	277	54	4'-11	277	54	4'-11	277		
5k2	PAVING NOTCH, TRANSVERSE		54	3'-5	193	54	3'-5	193	54	3'-5	193	54	3'-5	193	54	3'-5	193	54	3'-5	193	54	3'-5	193		
5p1	ABUTMENT HOOPS		112	10'-6	1227	112	10'-6	1227	100	10'-6	1096	120	10'-6	1315	112	10'-6	1227	96	10'-6	1052	96	10'-6	1052		
5p2	ABUTMENT EXT. HOOPS		-	-	-	-	-	-	-	-	-	-	-	-	24	10'-8	268	24	10'-8	268	24	10'-8	268		
6p3	ABUT. BOTT. AT PILES		-	-	-	-	-	-	-	-	-	-	-	-	28	6'-8	281	32	6'-8	321	36	6'-8	361		
5p4	ABUTMENT HOOPS AT ENDS		8	10'-9	90	8	10'-9	90	8	10'-9	90	8	10'-9	90	8	10'-9	90	8	10'-9	90	8	10'-9	90		
5r1	PAVING BLOCK LIFTING HOOPS		8	2'-10	24	8	2'-10	24	8	2'-10	24	8	2'-10	24	8	2'-10	24	8	2'-10	24	8	2'-10	24		
5s1	WING, VERTICAL		64	5'-10	390	64	5'-10	390	64	6'-2	412	64	6'-11	462	64	7'-8	512	64	7'-8	512	64	7'-8	512		
4f1	UNDER BEAMS AT ABUTMENTS		10	4'-8	32	10	4'-8	32	10	4'-8	32	10	4'-8	32	10	4'-10	33	10	4'-10	33	10	4'-10	33		
#2	PILE SPIRAL - NO. 2 BAR		10	38'-6	64	10	38'-6	64	12	38'-6	77	18	38'-6	115	20	38'-6	128	22	38'-6	141	22	38'-6	141		
	SPIRAL SPACER L <sub>1</sub> x L <sub>2</sub> x L <sub>3</sub> x 0.70		20	1'-10	26	20	1'-10	26	24	1'-10	32	36	1'-10	48	40	1'-10	53	44	1'-10	59	44	1'-10	59		
	REINFORCING STEEL (LBS.)				21269			23226			26196			32035			34651			36974			39303		
	SEE BARRIER RAIL DETAILS (LBS.)				4054			4410			4968			6002			6438			6875			7353		
	SEE OPEN RAIL DETAILS (LBS.)				4179			4572			5277			6816			7478			7900			8322		
	* VARIES FROM 4'-5 TO 29'-7 ** VARIES FROM 3'-0 TO 31'-0																								



CONCRETE PLACEMENT QUANTITIES (SUPERSTRUCTURE PLUS INTEGRAL ABUTMENTS)								ESTIMATED QUANTITIES (SUPERSTRUCTURE PLUS INTEGRAL ABUTMENTS)								
	46'-8	55'-0	67'-6	80'-0	90'-0	100'-0	110'-0	NO. OF STEEL H-PILES FOR TWO ABUTS. (HPIOX57)	NO.	10	10	12	18	20	22	22
SLAB, AND WITH BARRIER RAIL	CU.YD.	70.1	77.4	92.3	112.5	128.4	137.7	146.8	CU.YD.	106.4	113.7	129	157.8	174.5	183.8	192.9
ABUT. DIAPHRAGM WITH OPEN RAIL	CU.YD.	71.6	79.1	94.4	114.9	131.2	140.7	150.2	CU.YD.	107.9	115.4	131.1	160.2	177.3	186.8	196.3
PAVING BLOCKS	CU.YD.	2.5	2.5	2.5	2.5	2.5	2.5	2.5	L.F.	127.5	144.2	169.2	214.2	234.2	254.2	274.2
ABUTMENT WINGS	CU.YD.	7.2	7.2	7.6	8.4	9.2	9.2	9.2	LBS.	25,123	27,436	30,964	37,828	40,880	43,640	46,447
ABUTMENT FOOTINGS	CU.YD.	27.6	27.6	27.6	35.3	35.3	35.3	35.3	LBS.	25,448	27,798	31,473	38,851	42,129	44,874	47,625
									LBS.	1532	1526	1570	1610	1695	1695	1695
									LBS.	1242	1235	1235	1235	1265	1265	1265

LATEST REVISION DATE: 05-15

APPROVED BY BRIDGE ENGINEER: *Thomas E. McQuill*

**IOWADOT** Highway Division

STANDARD DESIGN - 30' ROADWAY, SINGLE SPAN BRIDGE

**PRETENSIONED PRESTRESSED CONCRETE BEAM BRIDGES**

APRIL, 2012

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**DECK & ABUTMENT REINF. H30SI-14-12**

15° SKEW