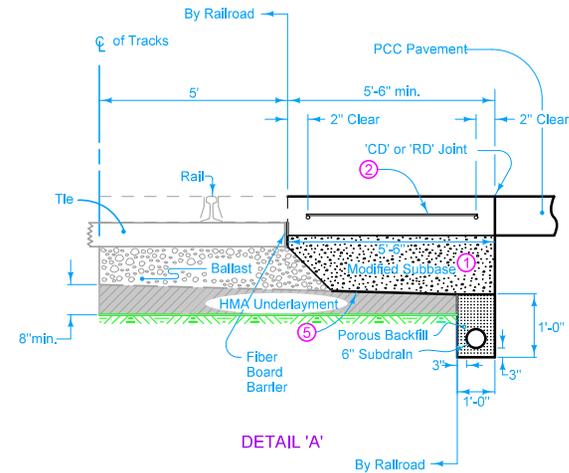
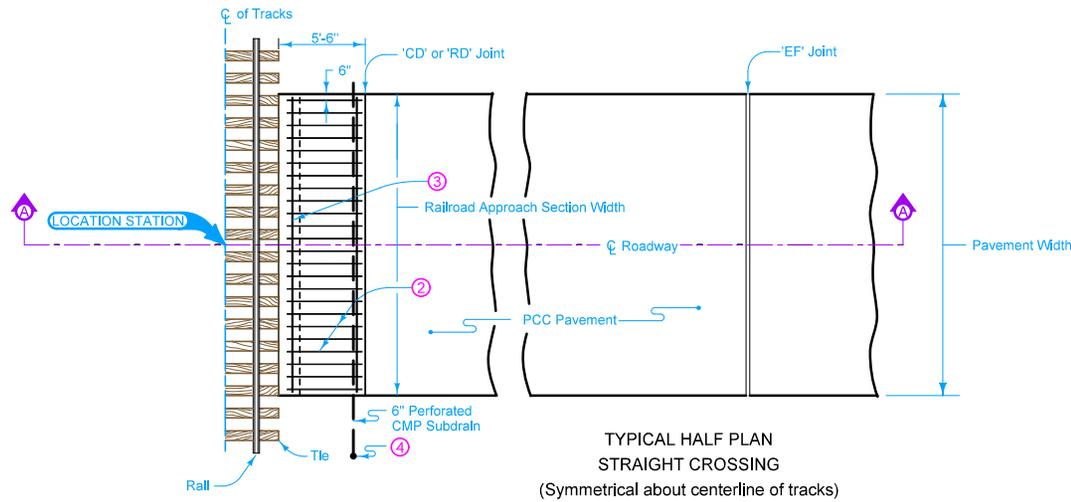


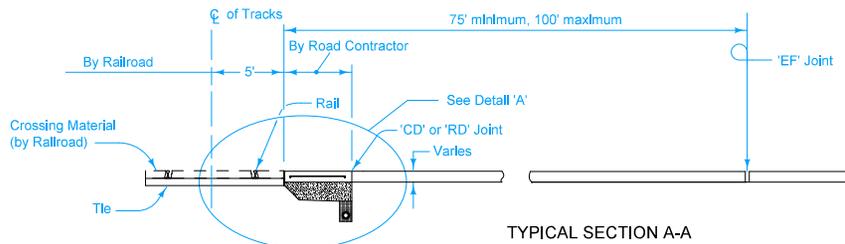
For joint details, see PV-101.

- ① Ballast meeting Railroad specifications may be substituted for modified subbase.
- ② #5 Bars at 12" centers located at half of the pavement thickness. Wire tie at all intersections with other bars. Lap a minimum of 1 foot when necessary and securely wire tie.
- ③ #5 Bars x (Approach Width - 4").
- ④ Outlet subdrain into ditch or storm sewer. See DR-303 and DR-304. Slope subdrain to drain.
- ⑤ Slope according to AREMA specifications



Possible Contract Item:  
Railroad Approach Section, P.C.C.

Possible Tabulation:  
112-3

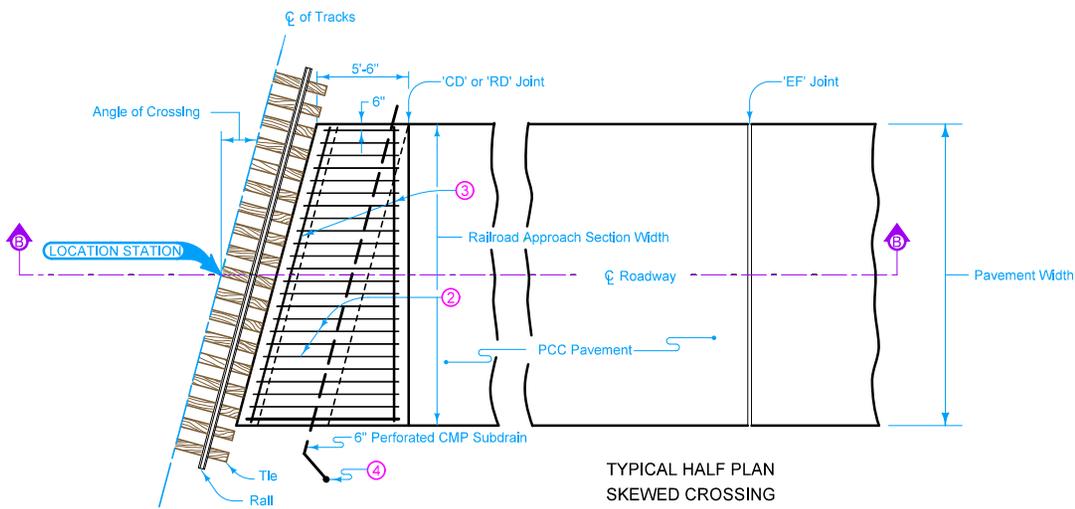


<b>STANDARD ROAD PLAN</b>	REVISION		
	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 10px; text-align: center;">1</td> <td style="width: 50px; text-align: center;">04-21-15</td> </tr> </table>	1	04-21-15
	1	04-21-15	
<b>PV-106</b> SHEET 1 of 2			

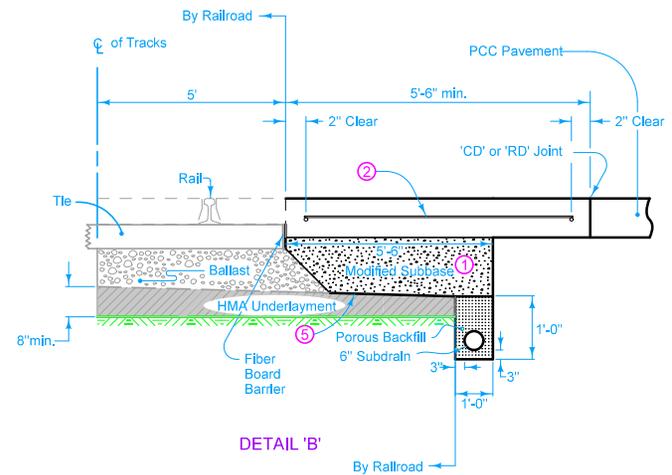
REVISIONS: Changed subdrain for skewed crossings to match skew. Replaced the DOT logo in the title block with the new version.

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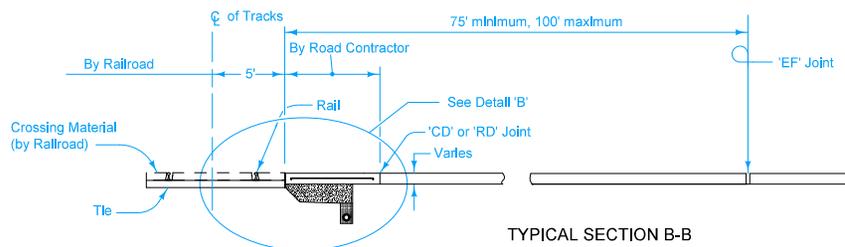
PCC RAILROAD  
APPROACH SECTION



TYPICAL HALF PLAN  
SKEWED CROSSING  
(Symmetrical about centerline of tracks)



- ① Ballast meeting Railroad specifications may be substituted for modified subbase.
- ② #5 Bars at 12" centers located at half of the pavement thickness. Wire tie at all intersections with other bars. Lap a minimum of 1 foot when necessary and securely wire tie.
- ③ #5 Bars x (Approach Width - 4").
- ④ Outlet subdrain into ditch or storm sewer. See DR-303 and DR-304. Slope subdrain to drain.
- ⑤ Slope according to AREMA specifications



TYPICAL SECTION B-B

 <b>STANDARD ROAD PLAN</b>	REVISION
	1   04-21-15
	PV-106
SHEET 2 of 2	

REVISIONS: Changed subdrain for skewed crossings to match skew.

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PCC RAILROAD  
 APPROACH SECTION